

## TRACK RULES

The rules and or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules.

NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATION OF OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official.

The race director shall be empowered to permit minor deviation from any of the specifications herein or impose any further restrictions that, in his opinion, do not alter the minimum acceptable requirements. NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH ALTERATION OF SPECIFICATIONS. Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.

### *The Management of Delaware International Speedway.*

#### **POINTS FOR HEAT RACE QUALIFIERS:**

|                             |           |
|-----------------------------|-----------|
| 1 <sup>ST</sup> Place ..... | 25 points |
| 2 <sup>nd</sup> Place ..... | 20 points |
| 3 <sup>rd</sup> Place ..... | 15 points |
| 4 <sup>th</sup> Place ..... | 10 points |
| 5 <sup>th</sup> Place ..... | 5 points* |
| 6 <sup>th</sup> Place ..... | 4 points  |
| 7 <sup>th</sup> Place ..... | 3 points  |

\*If three heats are needed only the top five places receive points.

#### **POINTS FOR FEATURE RACE QUALIFIERS:**

|                      |            |
|----------------------|------------|
| 1st Place .....      | 110 points |
| 2nd Place.....       | 100 points |
| 3rd Place.....       | 95 points  |
| 4th Place.....       | 90 points  |
| 5th Place.....       | 85 points  |
| 6th Place.....       | 80 points  |
| 7th Place.....       | 75 points  |
| 8th Place.....       | 70 points  |
| 9th Place.....       | 65 points  |
| 10th Place.....      | 60 points  |
| 11th Place.....      | 55 points  |
| 12th Place.....      | 50 points  |
| 13th Place.....      | 45 points  |
| 14th Place.....      | 40 points  |
| 15th Place.....      | 35 points  |
| 16th Place.....      | 30 points  |
| 17th Place.....      | 25 points  |
| 18th Place.....      | 20 points  |
| 19th Place.....      | 15 points  |
| 20th Place.....      | 10 points  |
| 21st Place.....      | 8 points   |
| 22nd Place.....      | 6 points   |
| 23rd Place.....      | 4 points   |
| 24th-30th Place..... | 2 points   |

#### **LEADER BONUS (FEATURES ONLY)**

A 10 point bonus will be awarded to any racer that leads a lap during a feature event. (This does not mean 10 points per lap led. It means you would get the 10 points bonus if you lead at least one lap during the feature.)

#### **TRACK RUN-IN BONUS**

Each racer will receive 5 points for participating in the track run-in. Transponders MUST be installed and operational to receive bonus.

## CONDUCT AND PROCEDURE RULES

1. No alcoholic beverages in the pits. Anyone caught will be barred from the pits, 2 weeks first offense; 1 year second offense.
2. Any driver found under the influence will be barred from driving for 1 year from date of suspension.
3. No working on or checking of race car on the track surface. Anyone doing so will be sent to the rear of the pack. Car may be worked on in the infield by driver or track officials only.
4. Anyone not obeying Flagman's signals may be black flagged from race track.
5. Any foul language or unsportsmanlike conduct on the race track will be black flagged to the pits.
6. Foul language or unsportsmanlike conduct will not be tolerated at the race track.
7. Any person entering another race team pit area for the purpose of causing a disturbance will be barred for 2 wks. 1<sup>ST</sup> offense - 1 yr., 2<sup>nd</sup> offense. All questions shall be directed to either the Pit Steward or the Del. State Police on duty.
8. "Black Flag" - Report to the pits for consultation. After 1 lap of black flag, the car will not be scored.
9. Any car running in an unsafe or hazardous condition will be black flagged.
10. Any driver getting out of his car to argue or question a call or decision of an official, will be sent to the pits for the remainder of the racing event that night, plus 1 week suspension or one racing event.
11. No fighting in the pits or on the track. First offense is suspension of 2 weeks, second offense is suspension from the track 1 year from date.
12. Any car that is not in his line up position on pit road or if he needs wrecker assistance will line up to the rear of the field.
13. The race will be started by the flagman only. If the lead car jump starts, or if he cannot set a sufficient pace, the driver will be warned the first time. If he cannot start at a proper pace, he will be moved to the rear of the field.
14. Any car jumping position before the green flag will bring out a caution and will be sent to the rear of the field. (Repeated jump starts will be black flagged.)
15. Charge Laps: There will be a charge lap on the start and on the next to last lap.
16. Accidents - Any car bringing out the caution or left sitting when the caution lights come on will line up to the rear of the pack, including the charge lap.
17. Line up after a caution flag. Line up on last completed lap or tower instruction.
18. After 1 lap has been scored, the starter will line the lead car in his position.
19. Caution Lights - All drivers must immediately bring their car to a slow pace and line up Delaware Start. Tower official will call out lineup over driver radios. Drivers should move immediately to posted positions, if they do not they will be sent to the rear.
20. Any car leaving the racetrack to pit while the race is stopped, will line up to the rear of the pack.
21. When signal from starter shows 1 lap to restart of race, all cars must be in their position. Any car in the infield after this signal will fall to the back of the pack.
22. Any feature restart past halfway will start single file. A Feature event that experiences difficulty to get laps complete may go single file at any time.
23. Any car bringing out or involved in 3 cautions during a Feature race will be black flagged to the pits.
24. When cars enter the pits, a safe speed must be maintained. Any driver found to be driving at an excessive speed or endangering pit persons will be held at pit road for consultation and can be penalized depending on the severity of the infraction.
25. Only car and driver allowed on pit road.
26. Any driver bringing out or involved in 2 cautions during Consi will be black flagged for the event.
27. Any driver that has not signed in on the insurance release form will not be paid for any position he finishes.
28. All protests must be made before the race by the owner or driver only.
29. In case of rain, the event will be considered complete when more than half of the event has been run. The remaining features will be run first on designated rain date.
30. Track Rules - All track rules should be read and understood by all owners, drivers and crew members. This is the responsibility of all crews. Any questions, please feel free to ask before you race.
31. **ANYONE THAT FEELS THAT ANY OF THE TRACK CONDITIONS ARE UNSAFE SHOULD LEAVE HIS CAR OUT OF COMPETITION.**
32. All classes, no mirrors
33. All classes, head and neck restraint recommended.
34. All bolt on weight MUST be painted white with car # & class CLEARLY displayed.

**DELAWARE INTERNATIONAL SPEEDWAY**  
**POLICY REGARDING USE OF/OR POSSESSION OF ILLEGAL DRUGS OR SUBSTANCES**

1. **ILLEGAL DRUGS, DEFINITION:** Illegal drugs are those substances defined and prohibited by state and/or federal laws.

2. **GENERAL PROHIBITION:** Possession or use of illegal drugs or drug substances, as defined above, is prohibited in any form, by any participant at Delaware International Speedway, either on the Speedway grounds, or in any area considered to be used in the operation of the Speedway such as parking lots or leased properties.

3. **PARTICIPANT, DEFINITION:** A participant is any person taking part in any event at Delaware International Speedway in any form, including but not restricted to drivers, car owners, mechanics, crew members, sponsors, track officials or pit area observers. All such persons shall be considered public figures who have, by their own choice, become involved in auto racing events at Delaware International Speedway, with the full understanding that he or she must abide by the rules and regulations established and published or announced by Delaware International Speedway. All participants are considered to be responsible for their personal conduct.

4. **VIOLATIONS AND PENALTIES:** Any person found to be in possession of or under the influence of an illegal drug or drug substance on the Speedway property, as defined above, OR any person who is arrested by duly-constituted authorities and charged with possession and/or use of an illegal drug or drug substances, OR any person who is formally charged by the court of law with illegal drug violations, **SHALL BE SUBJECT TO THE FOLLOWING PENALTIES BY THE SPEEDWAY:**

A. Suspension from competition and eviction from the Speedway property, and denial of further entry to the Speedway

for a period to be determined by Speedway officials.

B. Any participant who is formally charged by a court of law with an illegal drug violation, upon notification to Delaware

International Speedway management by that agency, shall be suspended from all forms of participation at Delaware

International Speedway until such time as the charges are fully adjudicated through the legal process. Any participant

convicted of a formal drug charge, by such process of law, will be prohibited from taking part in any Delaware

International Speedway events for a minimum period of 1 year from date of conviction.

5. **APPEALING AND HEARING:** Any participant suspended for violation of these rules may be granted an appeal hearing by a board of officials designated by Delaware International Speedway, provided the suspended participant requests such a hearing, in writing, within 14 calendar days of the date of suspension. It is the responsibility of the suspended party to make such a request if any hearing is desired.

6. **REINSTATEMENT:** A participant suspended for violation of these rules, **EXCEPT IN CASE OF PERSONS CHARGED WITH SELLING DRUGS**, may, as the result of a decision reached through the hearing process detailed above, be reinstated, if it is mutually agreed that the participant at his or her own expense -- will produce documentation from a physician licensed within the state, certifying that he or she is drug independent, as a result of random and periodical examinations and urinalysis testing, made at the request of Delaware International Speedway management.

7. **PRESCRIBED DRUGS:** If a participant is using prescription drugs on the advice of a physician, such use must be reported to the chief pit steward or racing director prior to the participant's entry into any Speedway activities. Failure to so notify will subject participant to penalties as described above.

## **QUALIFYING AND LINE UP PROCEDURES - ALL DIVISIONS**

### **QUALIFYING:**

1. Week one heat lineups will be by draw. Feature lineups will be heads up from heats. Following week one heat lineups will be by point average low to high.
2. If 24 cars or less are registered, there will be 2 heat races with 7 cars to qualify from each heat. Top 5 of those 7 cars will invert by point average low to high. All other cars will start behind the qualified cars in order of their heat finish.
3. If there are 25 or more cars, 3 heats will be run with 5 cars qualifying in each heat. Top 3 of those 5 cars will invert by point average low to high. All other cars will qualify through the consolation(s) and start in the order of their finish behind the cars that qualified through the heats.
4. Following week one, top 5 cars will be inverted by point average, low to high, the rest of the heat qualifiers will line up behind them in order of finish. Cars that qualify through the consolation(s) will line up in order of finish behind all qualified cars. Once a three Feature average is established, those cars will have priority over cars with two week averages, two week averages will have priority over drivers with only a one week's average and first time racers will start the heat at the rear.
5. Drivers who miss a week will not be handicapped by point average for the week they return and will start at the rear of the heat (or only ahead of a first time driver.) **All Classes**
6. There will be a minimum point average for inverted starts in all classes.

### **PROCEDURES:**

1. Any car that is not signed in at the pit tower will not start any racing event until they have signed in.
2. Any car that is not in his proper position on pit road or needs wrecker assistance starts in the rear.
3. Any driver may elect to start at the rear of the heat. Any qualified driver may elect to start at the rear of the feature.
4. Any driver that is currently in the top ten point's standings is allowed **one** provisional start a year, POINT SHOWS ONLY. They may use this provisional start if they do not qualify through the heat race or consolation. Max two cars per class. Provisional based on highest 2 drivers in points.
5. Any car or driver changes must be reported to the pit tower officials. Changes must be made before the event. Car must start at the rear of the field. If not reported before event, you will be deducted a maximum of 10 spots from finishing position.
6. Any car(s) that fail to meet post- race inspection will be positioned to the rear of the order of finish. Other sanctions or penalties are based on division rules.
7. A driver may not change cars during race including heats, consolations and features.
8. A driver can only compete in one division per night in a regular point program.
9. A driver can only qualify a second car in the same division through a consolation and by starting the consolation voids the finish of the first car.
10. Team cars must be clearly distinguishable from one another and use a different number or letter which is large enough to be easily seen from the tower.
11. Any current year champion will be a guaranteed starter in the Fall Championship race.
12. A driver that qualifies in the heat but breaks and cannot start the feature is still qualified and receives heat points.
- 13 Any driver banging or crashing into a fellow race car under caution may receive a 1 race suspension or a fine of \$250.00 or both.
14. All classes must have a Raceceiver or scanner to receive speedway broadcast. Raceceivers must be activated and in use during all events, including warm-ups. It is the responsibility of each race team to make sure Raceceivers are mounted safely in the cars and are in good operational order. Cars competing without working Raceceivers will be subject to fines and/or penalties, at the discretion of race officials. Drivers who do not follow instructions broadcast over the radio channel will be sent to the rear of the field or black flagged.
15. All classes must have AMB Trans x260 Transponder. The AMB Transponder is available thru the track or direct from AMB. All transponders must be registered with the pit tower prior to taking the track.
16. No car can race with rear bumper missing.
17. Make up race - Driver that has qualified can change car and start in qualified position.
18. One car cannot be entered in two classes at the same event. 1
19. If there is not a full field of cars, a non-qualified driver may start at the rear of the feature.

## 2017 MODIFIED LITE RULES

### BODY STYLES

- A. 1928-1948 American-made production vehicles only. 5/8 scale two-door sedans, coupes, or pickup trucks. These are to be known as **Classic Modified** body styles.
- B. Bodies resembling a Midwestern IMCA™ Modified shall compete as a DIS MOD LITE.
- C. Bodies resembling a DIRTcar™ Modified shall compete as a DIS MOD LITE

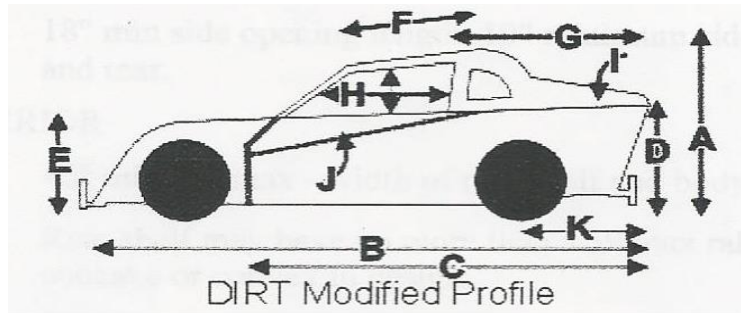
### BODY REQUIREMENTS AND SPECIFICATIONS

#### CLASSIC MODIFIED

- A. Body must be 5/8 scale, stock appearing. Maximum outside tire width 60". Car must pass freely through a hoop 60 .5" wide by 50 "high. 73 inch wheelbase plus or minus 1".
  - 1. Must have grill shell or simulated open radiator matching original body style, any material.
  - 2. Doors, windows, and cowlings must retain stock appearance. Hood louvers allowed for cooling, but must maintain stock contours.
  - 3. Engine compartment must conform to scale and match contour of body. Hole may be cut for breather clearance only.
  - 4. Must have closed trunk lid or full trunk access panel conforming to original body lines. Louvers allowed no holes.
  - 5. No fenders.
  - 6. No wings or spoilers.
  - 7. Ram air induction system prohibited.
- B. Body will be metal construction, 26 gauge steel or .040" aluminum minimum. Fiberglass or plastic grill shells allowed. All exposed edges must be folded or trimmed over. Legend cars are allowed. No lips or spoilers of any kind.
- C. NO open top cars (roadsters, convertibles) except for designated events. No panel trucks, station wagons, or specialty cars.
- D. Dimensions
  - 1. NO car over 50" tall, top to ground. Body height maximum 44", bottom edge to top (including frame rail).
  - 2. Maximum frame and body width: max 38" maximum frame width 34" min 30".
  - 3. Roof hatch entrance allowed.
  - 4. Frame ride height must not let car bottom out on track.
- E. Drivers doors must be operational and equipped with secure latch.
- F. Fasteners on hood, trunk, or panels must be positive, locking type and must be latched during competition.
- G. 124" maximum total length, bumper to bumper.
- H. Kick-outs or side nerf bars must be mounted at three points and run Full length between tires. 1.25" max .125 wall thickness. Cannot stick out beyond 1" from the side of the tire.

#### MOD LITE DIVISION

The intent of the following specifications is to follow the bodyline and design of an existing DIRT™ modified racecar. Any car specifications not covered in these body specs are subject to DIS interpretation Rulebook. Any significant performance advantage will be addressed on an individual basis. DIS reserves the right to determine what constitutes an unfair advantage.



## OVERALL DIMENSIONS DIRT™ MODIFIED DIVISION

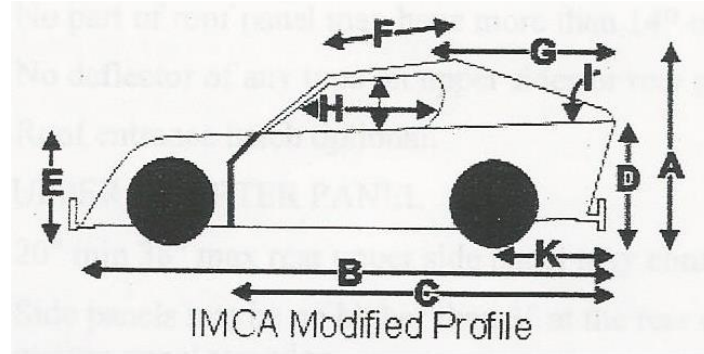
- A. 42" min. 50" max. from ground total car height .Body may be a max. of 44" from the bottom of the frame rail to the highest point of the roof.
- B. 128" max total length (bumper to bumper)
- C. **SIDES AND QUARTER PANEL** - 85" max - Door and rear quarter length. No part of the body may extend beyond the width of the rear tires.
- D. 30" min 38" max - side height of rear quarter panel from the ground 30" Max height of quarter panel.
- E. **HOOD** - 20" min 27" max - hood height excluding breather protectors (bottom of frame rail to top of hood) Ram air induction system prohibited. Rear of hood becomes part of windshield area rules. No part of nose section (in front of spindle centerline) may be wider than frame or a maximum of 24" Hood may taper out to door panels at the rear.
- F. **ROOF PANEL** F. 40" min 48" max - length of roof panel 36" min 42" max - width of roof No part of roof panel may have more than 14° of angle rake. No deflector of any type on sides or rear of roof panel. Roof entrance hatch optional. (except for designated events).
- G. **REAR UPPER QUARTER PANEL** 20" min 38" max - rear upper side panel may contain plexiglass Side panels may be no higher than 3" at rear of the straight line of quarter panel top edge. Side panel may be no higher than an imaginary line from the rear edge of roof panel to 3" above the top edge of the rear quarter panel (except for designated events).
- H. **SIDE WINDOW** 18" min side opening length. 10" min side opening height front and rear.
- I. **INTERIOR** - 46" min 50" max - width of rear shelf and body panels Rear shelf may have no more than a 5° max rake and may not be concave or convex in design. The rear frame area from the rear shelf downward a min. 16" must be completely enclosed the width of the frame. Louvers allowed but no holes.
- J. **ADDITIONAL INTERIOR SPECIFICATIONS**
1. 36" min 56" max - length of right side pod area with 30° max angle. Left side pod may be altered for driver access.
  2. Cockpit cover with a max off ½" lip will be permitted providing the on/off cutoff switch is reachable from outside the car by safety personnel.
  3. Deck lid height from ground 30" min.- 36" max. with NO rear lip extending upward.
  4. Side pod may not extend rearward past the centerline of rear axle.
  5. Body must be centered on the chassis.
  6. If windshield is used, it must be flat.
  7. All body dimensions will be teched + or - ½" unless otherwise specified.
- K. Rear quarter distance from center of rear 20" min 30" max.

## L. REAR SPOILER

1. Rear spoiler must be a single blade constructed of aluminum or Lexan mounted to the interior body shelf and be no wider than the inside of the rear quarter panels with a 50" max.
2. The spoiler can be no higher than 4" measured from the shelf.
3. May be mounted at any angle but may not extend beyond or above the rear quarter panel and may not be hinged or adjustable.
4. No bracing or supports on top, front or back of the spoiler blade permitted.

## IMCA MOD LITE

The intent of these preceding specifications is to follow the bodyline and design of the Midwestern IMCA™ modified racecar.



## IMCA MOD LITE

### OVERALL DIMENSIONS OF IMCA™ MOD LITE

- A. 42" min. 50" max. from ground – total car height. Body may be a maximum of 44" from the bottom of the frame rail to the highest point of the roof of the car.
- B. 128" maximum total length, bumper to bumper.

### C. SIDES AND REAR QUARTER PANELS

1. 85" max. body length from front door edge to rear quarter panel including rear spoiler and mounts.
  2. Body side panels – lower rear wheel opening may taper out towards the outside edge of the rear tires. Maximum width can be no more than 58". Side panels must maintain 50" maximum width at the front edge of the doors, top edge of doors and rear edge of quarter panel.
  3. Side nerf bars may be mounted on the inside or outside of the door panels.
- D. 30" min. 38" max. side height of rear quarter panel from the ground. 30" max. height of quarter panel.

### E. HOOD

1. 20" min. 27" max. hood height excluding breather protectors (bottom of frame rail to top of hood). Ram air induction systems prohibited. Rear of hood becomes part of windshield area rules. No part of the nose section (in front of spindle centerline) may be wider than a maximum of 24"
2. Hood may taper wider from the center line of spindles to the out front edge of the front door panels.

### F. ROOF PANEL

1. 40" min. 48" max – length of roof panel. 36" min. 42" max. – width of roof.
2. No part of the roof panel may have more than 14° of angle rake.
3. No deflector of any type on upper sides or rear of roof panel.
4. Roof entrance hatch optional.

## **G. REAR UPPER QUARTER PANEL**

1. 20" min. 38" max. rear upper side panel may contain Plexiglas.
2. Side panels may be no higher than 3" at the rear of the straight line of quarter panel top edge.
3. Side panel may be no higher than an imaginary line from the rear edge of the roof panel to 3" above the top edge of the rear quarter panel.

## **H. SIDE WINDOW**

1. 18" min. side opening length. 10" minimum side opening height front and rear.

## **I. INTERIOR**

1. 46" min. 50" max. – width of rear shelf and body panels.
2. Rear shelf may have no more than a 5° max. rake and may not be concave or convex in design.
3. No lips or spoilers of any kind.
4. Louvers allowed – no holes.

## **J. ADDITIONAL INTERIOR SPECIFICATIONS**

1. Side pod and rear shelf location must follow the same line and be level with the top edge of the door panels and not extend beyond the front edge of the door or beyond the rear edge of the rear quarter panel.
2. Cockpit cover with a max off ½" lip will be permitted providing the on/off cutoff switch is reachable from outside the car by safety personnel.
3. Deck lid height from ground 30" min.- 36" max. with NO rear lip extending upward.
4. Side pod may not extend rearward past the centerline of rear axle.
5. Body must be centered on the chassis.
6. If windshield is used, it must be flat.
7. All body dimensions will be teched + or – ½" unless otherwise specified.

K. Rear quarter distance from center of rear 20" min. – 30" max.

## **L. REAR SPOILER**

1. Must be single blade lexan or aluminum and mounted to the rear of the interior of the body shelf and be no wider or higher than the inside of rear quarter panels. 50" max.
2. Maximum width of spoiler may not exceed 4".
3. May be mounted at any angle but may not extend beyond or above the rear quarter panel and may not be hinged or adjustable.
4. No bracing or supports on top, front, or back of spoiler blade permitted.

## **BUMPER DIMENSIONS**

A. Front maximum width 30". Bumper height 6" to 8" with 2-bar loop. Ground to top of bumper: 12" minimum, 16" maximum. Bumper may not extend more than 12" forward of front tires.

B. Rear maximum width 56", with 3 vertical bars. Bumper height 6" to 8" with 2-bar configuration. Ground to top of bumper: 12" minimum, 18" maximum. May not extend more than 8" behind rear body panels.

C. Bumpers must be hollow with maximum wall thickness of .125"

## **NERF BARS**

Full length nerf bars between front and rear tires mandatory. Maximum outside Diameter of 1.25" and .125 max. thickness. Must be hollow. May not extend out beyond tread width more than 1" at the rear only.

## **FIREWALL**

Firewall must be metal and must completely separate engine compartment from cockpit. Engine protruding into cockpit must be enclosed.



## **WINDSHIELD**

Windshield must be Lexan, wire mesh, or protection bars. No Glass. All other cockpit areas must remain open. Deflectors may be used in front of driver and may be no wider than roll bar area.

## **SEAT**

Seat must be high-backed aluminum racing type. All cars will have a permanent bar located behind top of seat at shoulder level. Bottom of seat will not be more than 65" back measured from upper ball joint. Seat may be tilted back for added driver head clearance, NO portion of the seat may be back more than 74" as measured above.

## **NUMBERS AND GRAPHICS**

Car number must be minimum 12" in height and 2" stroke. Must be displayed on each door and roof or trunk. Numbers in 4" size should be on front and rear of car to help with line-ups.

## **WHEELS AND TIRES**

- A. Steel wheels only, minimum wall thickness .090", 13" only. Wheel width max. 8".
- B. Hoosier Racing Tires plated Stars-S, Stars-M or TUSA-S, TUSA-M Allowed. .40-Durometer min. before race.
- C. Tires may not be altered from the original construction properties as per rubber consistency or tire construction specifications.
- D. Bead lock rims are optional.
- E. Tires may be grooved, scythed and or buffed.

## **WEIGHT**

- A. All Cars with ZX12, 1040, 1070, & 999 Open programs must weight a minimum of 1340 lbs. with driver after race.
- B. All cars with Spec Engines must weigh 1325 min. with driver after race.

## **999 OPEN ENGINES**

Engines eligible for this class are: Suzuki GSXR1000, Honda CBR1000, PR1000, Yamaha YZFR1-1000, Kawasaki 2X10-1000 up to 2013 model year.

- A. Must remain stock stroke and rod length.
- B. Engines will be limited to 3 mm bore increase.
- C. OEM Fuel Injection must remain stock appearing.
- D. All head components must remain OEM with the exception of cams, cam sprockets, and valve springs. NO PORTING OR POLISHING.
- E. Only one fuel-monitoring device wired into the harness with no other connections will be allowed during competition. No fuel monitoring control device allowed in the driver's compartment or within driver's reach.
- F. Fuel – gasoline, ethanol or methanol permitted. NO nitro, nitro blends or nitrous oxide allowed.
- G. Aftermarket air box with optional velocity stacks are allowed. No ram type air induction.
- H. No dry sumps allowed.
- I. Oil Pan may be aftermarket or cut modification allowed.
- J. OEM ECU box only, one additional fuel management system allowed that has no other capabilities.
- K Cylinder cranking compression: minimum 100 PSI – maximum 220 PSI per cylinder average of all 4 cylinders. NO variance.

## **KAWASAKI ZX12**

- A. Must remain stock bore, stroke and rod length.
- B. No porting or polishing of cylinder head.
- C. Port matching to Intake Boot allowed, not to exceed 3/8" into port.
- D OEM cams only.
- E. Stainless steel valves allowed.
- F. OEM fuel injection must remain stock appearing.
- G. Fuel: gasoline, ethanol and methanol permitted. NO nitro, nitro blends or nitrous oxide allowed.
- H. OEM ECU box with one additional fuel monitoring system allowed.
- I. Oil pan may be aftermarket or cut modification allowed. NO dry sump systems allowed.
- J. Cylinder cranking compression: minimum 100 PSI – maximum 220 PSI per cylinder average of all 4 cylinders. NO variance.

## **GENERAL ENGINE SPECS**

- A. Displacement maximum:
  - 1. Side draft motors 1250cc. (+1%)
  - 2. Down Draft motor limited to 1200cc maximum with no variance.
  - 3. Water-cooled engines 1100cc and below can run carburetors or fuel injection systems. Engines above 1100cc OEM must remain in their original stock configuration for carburetors or fuel injection.
  - 4 No engine can be modified to be less than original stock configuration OEM specifications.
  - 5. OEM electronic engine components (fuel management type systems allowed).
  - 6 Carburetors and cams optional for oil and air cooled engines.
- B. Four cycle maximum engines only.
- C. Engines must have a working transmission, clutch and starter with all parts in place. Complete clutch-assembly must remain in engine. External reverse boxes allowed. No air controlled shifting devices.
- D. Charging system optional. Battery powered ignitions systems only. Alternator cover allowed.
- E. No auxiliary starters allowed.
- F. Engines must be front mounted in engine compartment.
- G. Engine setback: The "square of the engine" (rear fins of the jugs) may not extend rearward more than 17" from the center of front spindle. No part of the engine or transmission may extend rearward more than 21 inches from center of front spindle.
- H. OEM fuel injection must remain stock appearing.
- I. Engine must be cooled by original intent and may use extra fan or oil cooler.
- J. **Aftermarket Cylinder & Cylinder Heads:** No aftermarket cylinder & cylinder heads allowed on water-cooled motors. No aftermarket cylinder heads allowed on oil & air cooled motors, aftermarket cylinders authorized.
- K. Porting, grinding, polishing or changing stock configuration of intake or exhaust ports allowed for oil & air cooled motors. Carburetor/Fuel injection boot must fasten directly to head in the stock intake port location.
- L. No porting, grinding, polishing or changing stock configuration of intake or exhaust ports allowed on water cooled motors. Casting part lines must be visible upon inspection. Intake Port area may be altered to match intake boot. Alterations not to exceed 3/8" into intake port. Carburetor/Fuel injection boot must fasten directly to head in stock intake port location.
- M. Header must attach directly to head in stock exhaust port locations.
- N. Oiling system may not include a dry sump system.
- O. No increasing or decreasing of motor stock stroke in any way.
- P. **MUFFLERS ARE MANDATORY.** Only NAPA Part No. 18169 and optional Lobac DIS stamped muffler (Lobac can be purchased from Musser Racing 302-875-7461) can be used. **ANYONE WHO TAMPERS OR MODIFIES MUFFLER IN ANY WAY WILL BE DISQUALIFIED!** It is recommended that the exhaust exit on the side of the car.

Q. Maximum cylinder cranking compression 220 psi. per cylinder minimum 100 psi average 4 cylinders, no variance, measured with a Snap-On compression gauge, part number EEPV303A, with adapters EEPV304A, EEPV306A or MT26J200, after ten (10) revolutions of cranking, 15-45 minutes following feature event.

R. Carburetors or Fuel Injection and all 4 spark plugs must be removed before checking compression.

## **BATTERY & FUEL SYSTEM**

A. Battery and fuel pump must be mounted outside driver compartment. Battery must be secure and held with non-rubber strap. Electric fuel pump allowed.

B. Fuel cell mandatory, maximum 5 gallons, located in trunk.

C. Gasoline, ethanol or alcohol only. No power enhancing additives allowed.

D. Reinforced or steel braided line must be used in all high-pressure areas.

E. A direct manual cutoff switch wired to ground circuit, will be installed on the right rear shelf or rear fire wall within 6" of the corner formed by the right shoulder of the driver.

The emergency electrical cut off switch will be clearly located and marked.

## **DRIVE TRAIN**

A. Must use steel driveshaft from transmission to automotive rear end. Minimum 2.50" x .083" or 2" x .120". Should be painted white.

B. Quick change rear ends allowed.

C. Drive shaft tunnel must have minimum of 2 (3 recommended) 360 degree hoops between firewall and seat back. Must be fabricated of minimum .1875" x 1.5" steel bar. 1" x .095" steel tube highly recommended.

D. Rear must be lock or have a solid spool.

## **FRAME AND ROLL CAGE**

A. Main frame members will be minimum of 1" x 2" x .120" wall rectangular steel tubing only. Mainframe rail consist of the length between the front lower a-arm cross member and the rear main roll bar upright. No aluminum allowed in cage or frame.

B. Main frames will have a minimum of 3 cross members between firewall and back of seat fabricated of same material as frame rails. Alternate material may be steel angle, min. .125" x 1.5" x 2". Cross bracing in floor under driver strongly recommended. Rear frame rails at rear roll bars must be 34" max and 30" min outside dimension. Rear roll bar must meet same dimensions as the frame it is mounted to. Frame may taper forward of driver seat area. Front engine area frame width must be 24" max and 20" min outside dimension. Left and right frame rails must be the same + or - .

C. Roll cage must be DOM steel minimum 1.25" x .083" (.095 recommended) in all cars manufactured from 12/98 on. Clubs may grandfather older cars with 1" x .065" tubing Cross-brace or diagonal strongly recommended in hoop over driver's head.

D. All cars must have cage bars installed across door openings on both sides. Bars should be welded in place as part of roll cage. Minimum size 1" x .065". (1.25" x .095" recommended) Cars with bars mounted on doors should have full door frame and minimum of 2 horizontal cross bars. Minimum 1" x .083". Cars with bars mounted on doors must have 1/4" x 2" steel stops welded in on each side of opening, totaling a minimum of 4" on each end of door.

E. It is required to have a steel bar with a minimum size of 1" x .065" from door post to door post, under the dash. Steel plate on driver's door bars recommended.

F. It is recommended that, if using rivets, body panels attach to tabs welded onto cage instead of drilling multiple holes into cage bars.

G. No down tube frame cars allowed. Any bars going from cowl area to snout must remain below the upper line of hood.

H. Classic Pickups may have 2 bars going from upper rear roll cage down to rear clip.

## **SUSPENSION**

- A. Front suspension must not extend rearward past the most forward panel of the firewall. Coil over shocks must be attached directly to the lower A arm and upper chassis.
- B. Rear trailing arm/radius rod specifications:
  - 1. Arms may have a max length of 30"
  - 2. Torque-absorbing devices are permitted.
  - 3. Maximum 3 radius rods or 3 link suspension.
  - 4. No cantilever front or rear suspensions permitted.
- C. Wheel offsets are allowed.
- D. Hub offsets are NOT allowed. Hubs must be within .5" distance from main frame rail on either side of car. Measured from center of rotor to vertical plane of frame rail.
- E. Coil-over shocks are the only allowable means of supporting the chassis. One shock and spring per wheel only.
- F. Shocks must be Small Body Steel Shock only and must all-steel construction. No Progressive Springs allowed.
- G. No driver shall have the ability to adjust suspension from inside car.
- H. Straight front axles are prohibited. Local clubs may grandfather existing cars for competition.
- I. No leaf springs or torsion bars allowed.
- J. No bird-cage type rear linkage.
- K. Anti-roll (sway) bars not permitted front or rear.
- L. Frame rod height must not let car bottom out on track.
- M. The use of carbon fiber or titanium in ANY WAY is prohibited.

## **STEERING**

- A. All steering components must have safety fasteners such as cotter pins or self-locking nuts.
- B. Manufactured quick-release steering wheel hubs mandatory.
- C. Rack and pinion steering is recommended.

## **BRAKES**

- A. All four wheels must have working calipers and rotors.
- B. Steel rotors mandatory,
- C. Adjustable brake bias allowed.
- D. Competition-type master cylinders allowed.

## **SAFETY EQUIPMENT**

- A. Fire bottle system HIGHLY recommended.
- B. Driver uniform will consist of:
  - Firesuit (single layer minimum, one or two-piece)
  - Fireproof gloves
  - Leather or fireproof shoes
  - Approved helmet with Snell 85 or higher.
  - Full face helmet highly recommended if using wire mesh windscreen.
  - Neck collar
- C. Car must be equipped with 5-point safety harness, with minimum of 3 inch belts.
- D. Window nets or arm restraints HIGHLY recommended.

## **SCORING**

- A. All cars must have AMB Transponder